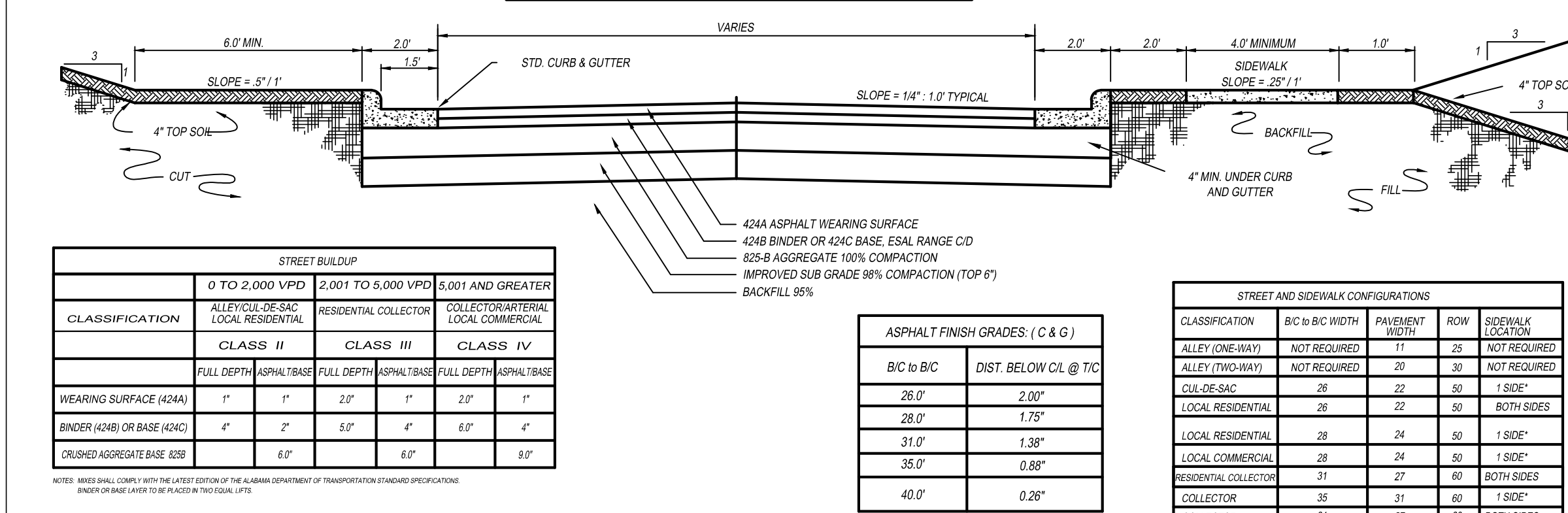
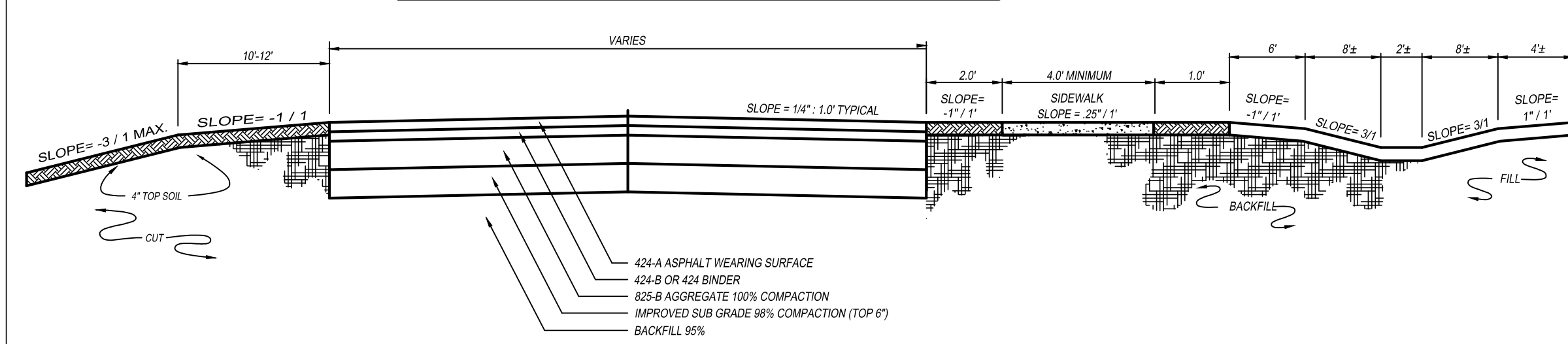


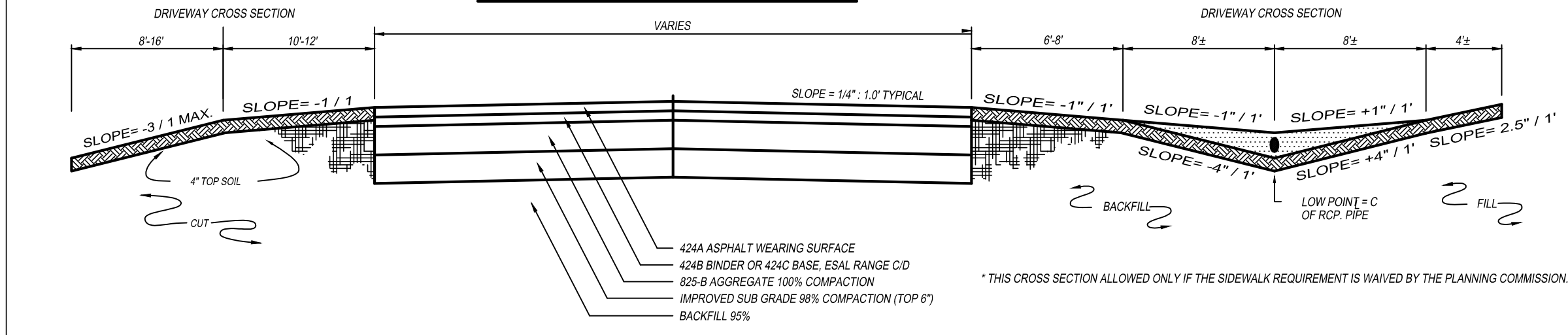
TYPICAL STREET CROSS SECTION WITH CURB/GUTTER AND SIDEWALK



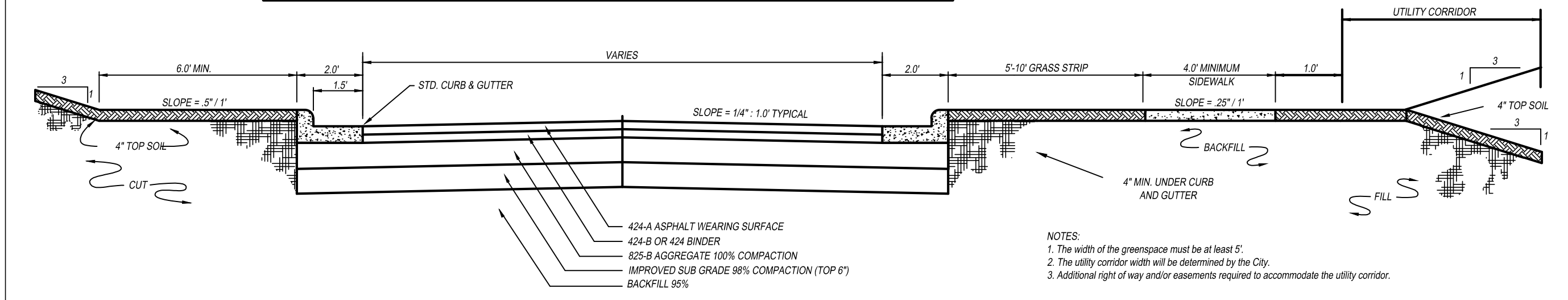
TYPICAL STREET CROSS SECTION WITHOUT CURB/GUTTER, AND WITH SIDEWALK



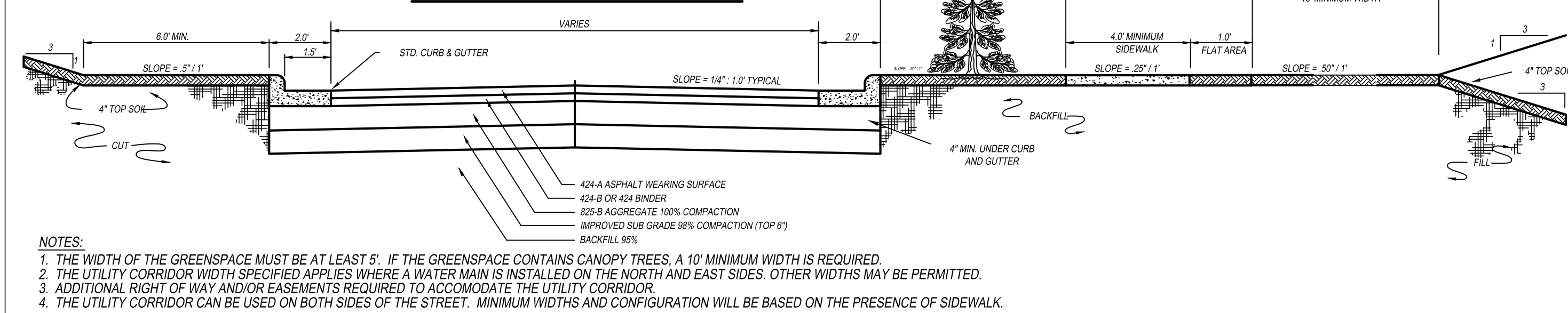
TYPICAL STREET CROSS SECTION WITHOUT CURB AND GUTTER



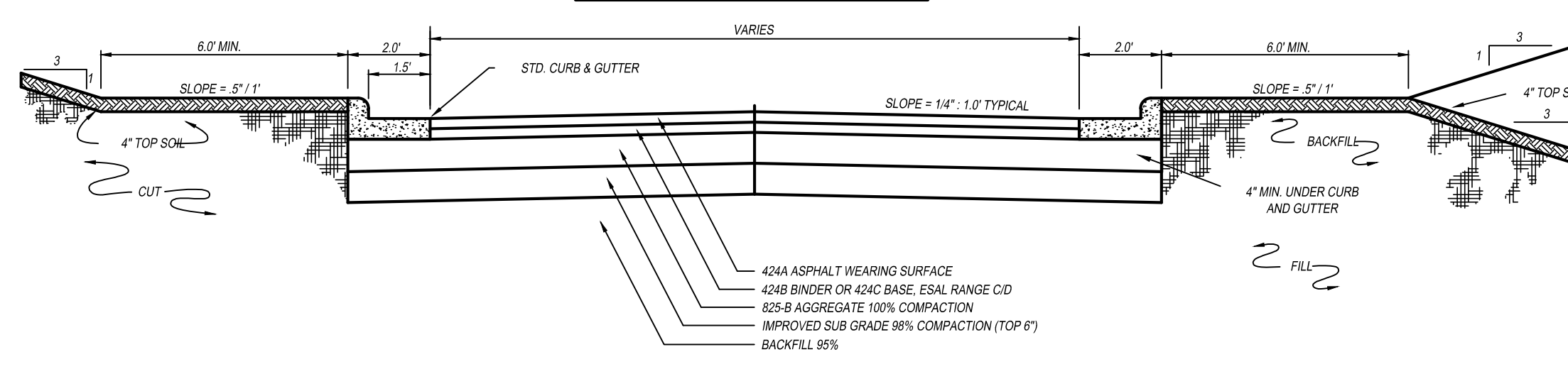
TYPICAL STREET CROSS SECTION WITH A UTILITY CORRIDOR



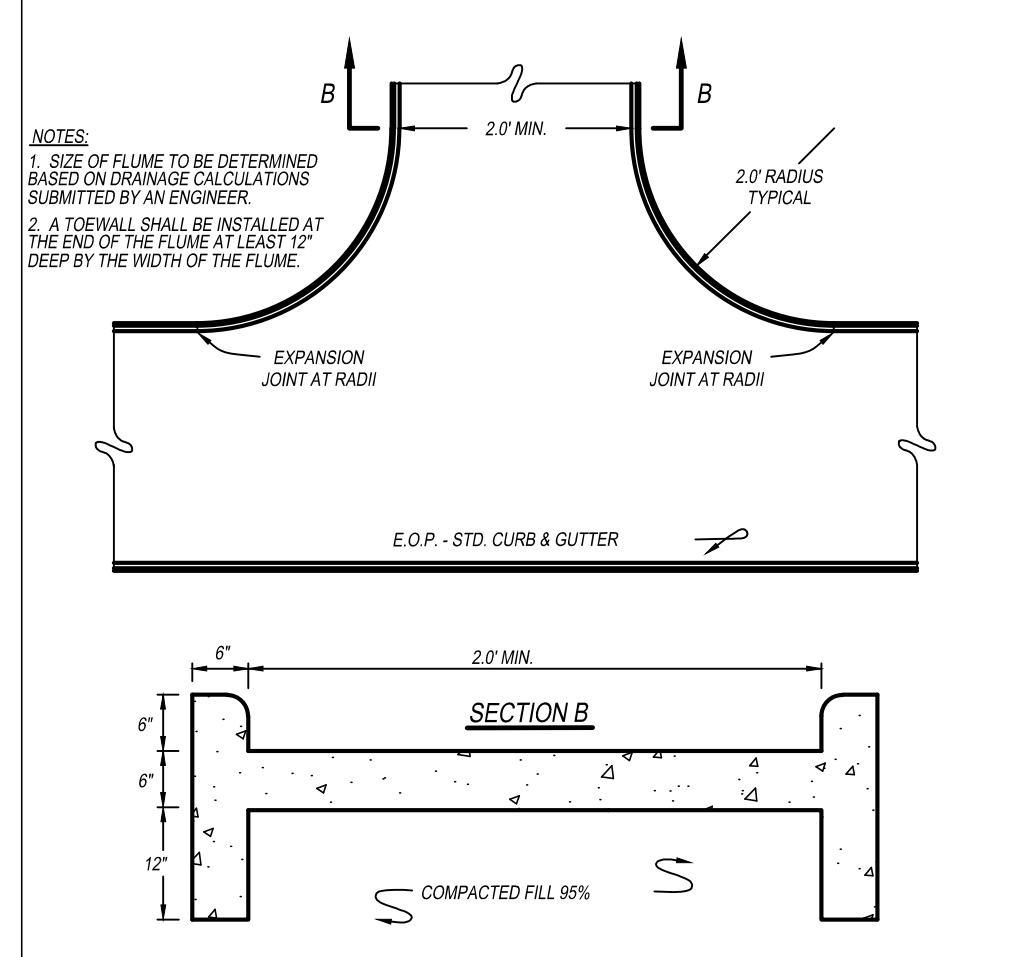
TYPICAL STREET CROSS SECTION WITH A LANDSCAPE CORRIDOR



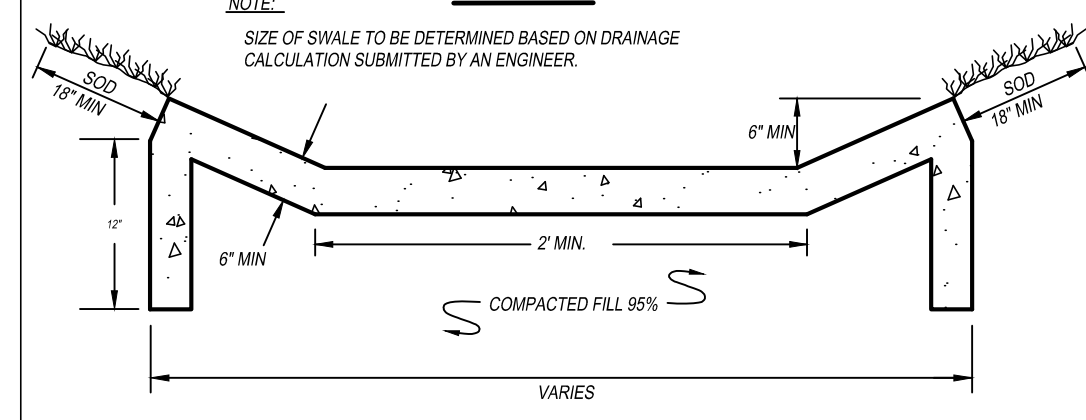
TYPICAL STREET CROSS SECTION WITH CURB AND GUTTER



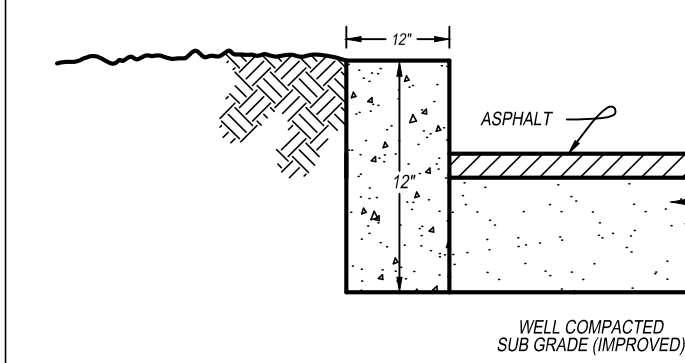
FLUME DETAIL



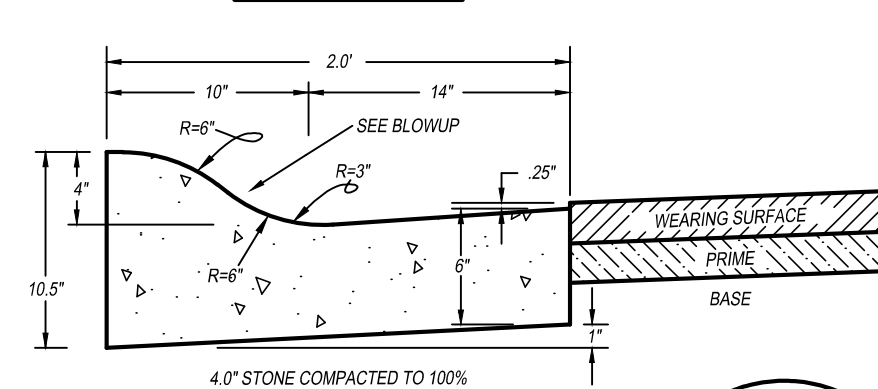
SWALE



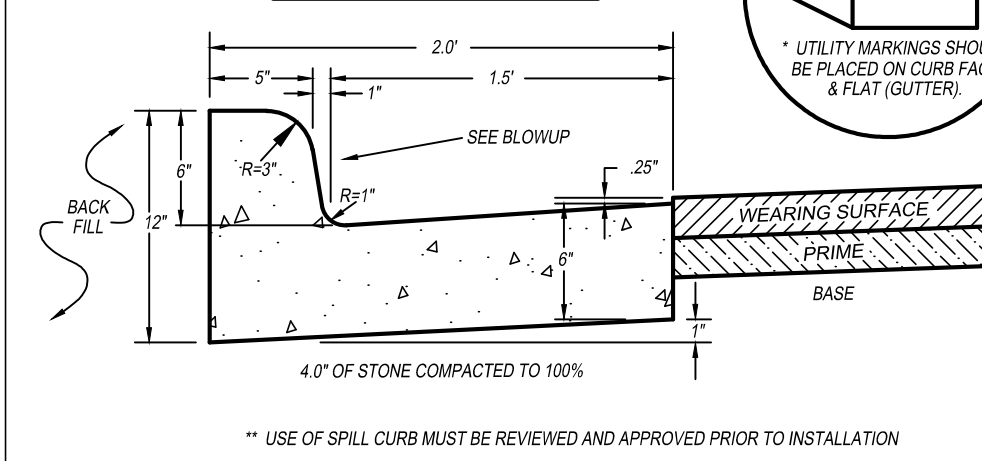
HEADER CURB



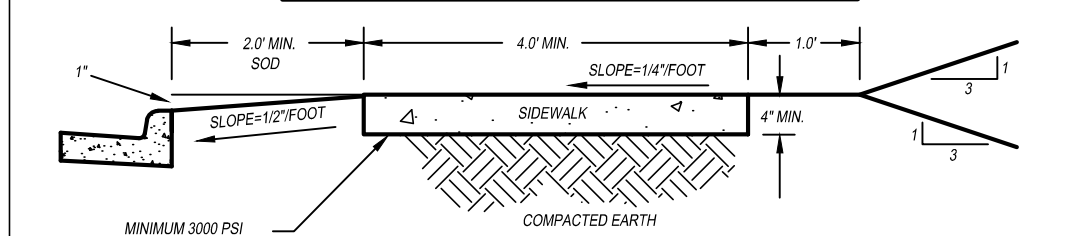
ROLL CURB



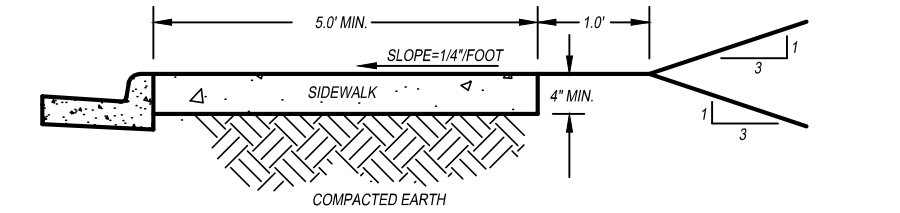
CURB & GUTTER



SIDEWALK with GREENSPACE *

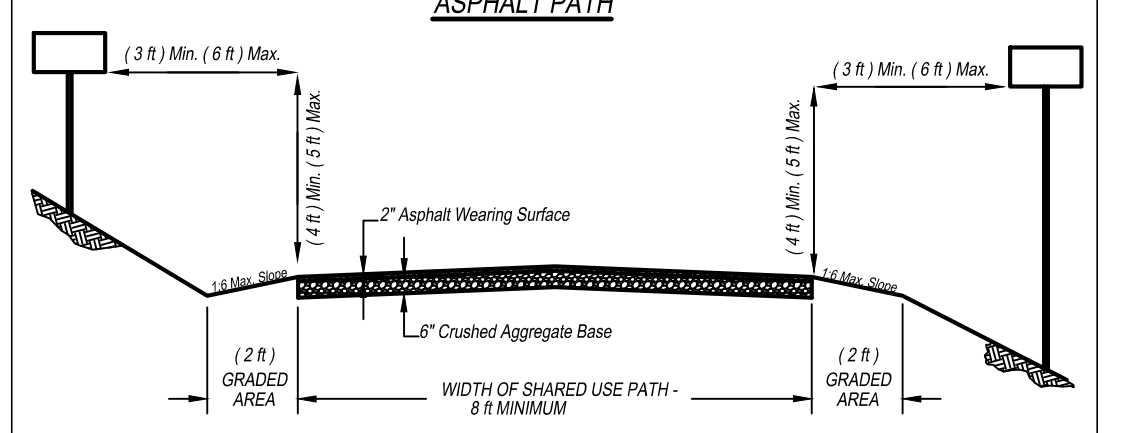


5' SIDEWALK

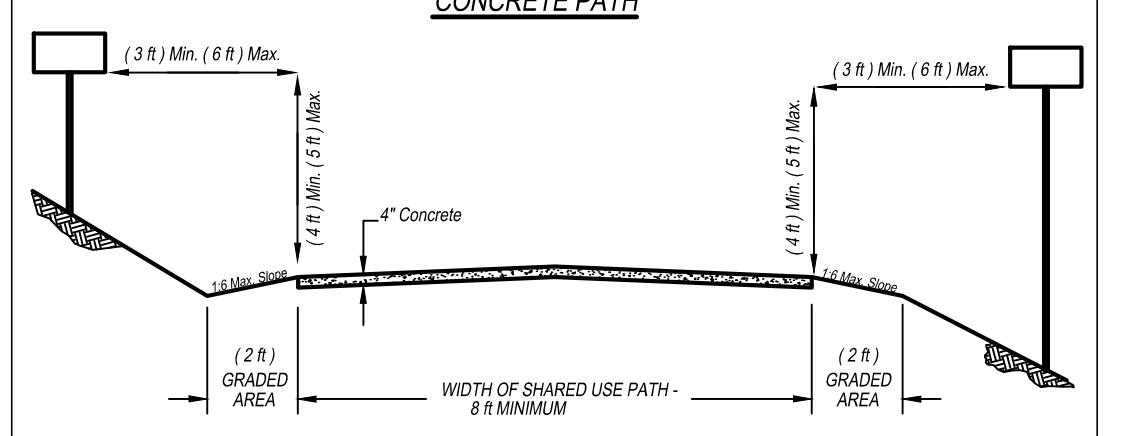


- NOTES:
1. PROVIDE 1'-1/2" DEEP BY 1/8" WIDE CONTROL JOINTS EVERY 5' WITH EXPANSION JOINTS EVERY 50'. EXPANSION MATERIAL CAN BE FILTER BOARD OR TREATED 1/4"
 2. WHEN THE DISTANCE BETWEEN THE BACK OF CURB AND THE EDGE OF SIDEWALK IS LESS THAN TWO FEET, THE SIDEWALK MUST BE A MINIMUM OF FIVE FEET WIDE.
 3. SIDEWALK ACCESSIBLE ROUTES WITH CLEAR WIDTH LESS THAN 60 INCHES SHALL PROVIDE PASSING SPACES AT INTERVALS OF 200 FEET MAXIMUM. PASSING SPACES SHALL BE EITHER: A SPARE 60 INCHES BY 80 INCHES MINIMUM, OR AN INTERSECTION OF TWO WALKING SURFACES PROVIDING A T-SHAPED SPACE COMPLIANT WITH PROWAG STANDARDS.
- * 4 FOOT WIDTH IS ALLOWED ON LOCAL STREETS AND CUL-DE-SACS. 5 FOOT MINIMUM IS REQUIRED ON ALL ARTERIALS, COLLECTORS, LOCAL COMMERCIAL, AND RESIDENTIAL COLLECTORS.

CROSS SECTION OF TWO-WAY SHARED USE PATH ON SEPARATED RIGHT-OF-WAY

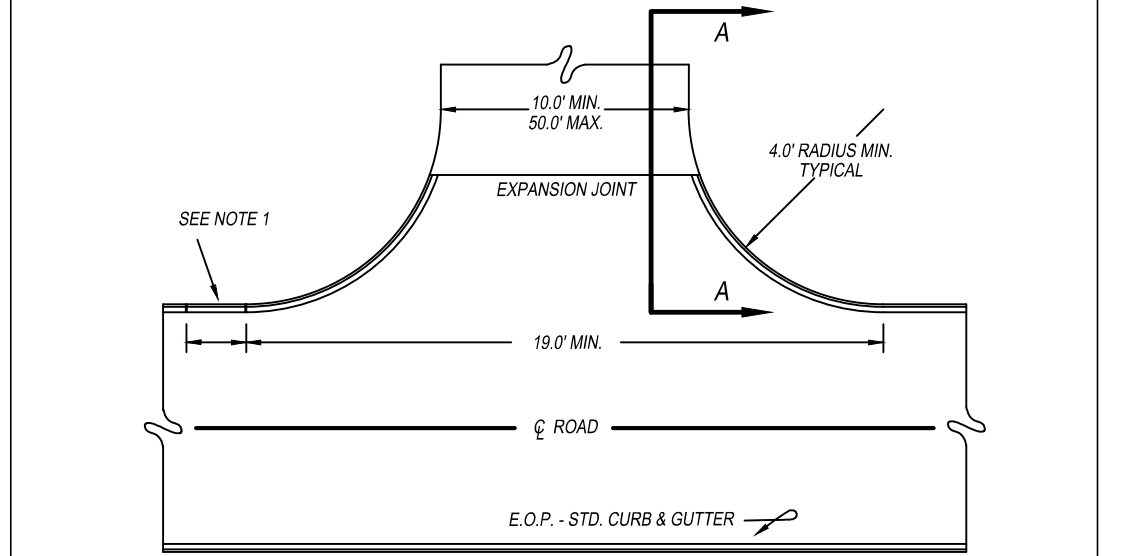


CROSS SECTION OF TWO-WAY SHARED USE PATH ON SEPARATED RIGHT-OF-WAY

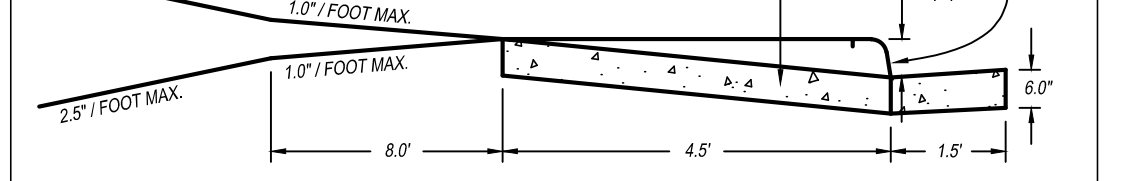


STANDARD DRIVEWAY

- NOTES:
1. DISTANCE FROM RADIUS POINT TO EXISTING EXPANSION OR CONSTRUCTION JOINT SHALL BE AT LEAST 3'.
 2. IF THE DISTANCE IS LESS THAN 3', CURB & GUTTER SHALL BE REPLACED TO THE EXISTING JOINT.
 3. EXPANSION JOINT TO BE PLACED AT THE IN.
 4. THE TEN FOOT MINIMUM WIDTH IS FOR RESIDENTIAL USES ON LOCAL STREETS, CUL-DE-SACS, AND ALLEYS.
 5. ALL OTHERS SHALL BE TWELVE FOOT MINIMUM WIDTH. THE 50' WIDTH IS RESERVED FOR COMMERCIAL AND MULTI UNIT RESIDENTIAL DEVELOPMENTS.
 6. DRIVEWAY TURNOUT WIDTHS ARE MEASURED AT THE RIGHT OF WAY.
 7. REMOVE CURB & GUTTER FOR DRIVEWAY TURNOUT PLACEMENT. SAW CUTTING IS PERMITTED ALONG THE CURB LINE. GUTTER TO MAINTAIN EXISTING GUTTER.
 8. ALL CONCRETE SHALL BE A MINIMUM OF SIX INCHES THICK.
 9. RADI FOR USES OTHER THAN RESIDENTIAL MUST BE TWENTY-FIVE FOOT MINIMUM.
 10. ON STREETS WITH SIDEWALK CONCRETE DRIVEWAY TURNOUT MUST EXTEND TO THE BACK EDGE OF THE SIDEWALK.
 11. CONCRETE DRIVEWAY TURNOUT MUST MEET CITY STANDARDS OR CAN BE DESIGNED TO SITE SPECIFIC CONDITIONS.



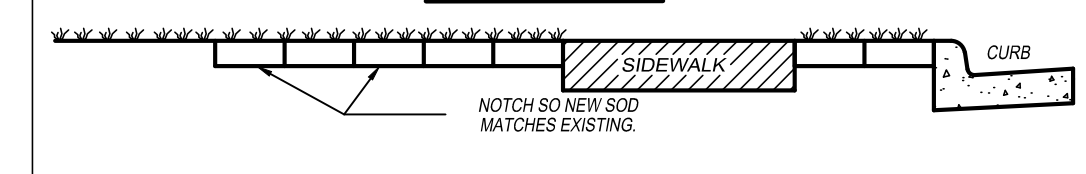
SECTION A



NOTES:

1. SIDE SLOPES FOR STREETS SHALL VARY FROM A POINT SIX (6) FEET BEHIND THE CURB TO THE EXISTING ELEVATION AT THE AT THE RIGHT OF WAY (R.O.W.), EXCEPT THAT SUCH SLOPE SHALL NOT BE GREATER THAN 3:1. IN CASES WHERE A 3:1 SLOPE CARRIES THE CONSTRUCTION LIMITS BEYOND THE R.O.W. LINE, THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY CLEARING & GRUBBING, EXCAVATION, BACKFILL, MULCHING OR ANY OTHER WORK REQUIRED TO ACCOMMODATE THE 3:1 SLOPE. IN CASES WHERE ROCK IS ENCOUNTERED, THE SLOPE MAY BE 2.5:1 IN THE ROCK PORTION.
2. FOR PORTLAND CEMENT CONCRETE PAVEMENTS, THE TYPICAL CROSS SECTION SHALL BE DESIGNED ON A CASE BY CASE BASIS.
3. CURB & GUTTER SHALL BE CAST IN PLACE WITH THE FOLLOWING REQUIREMENTS: EXPANSION JOINT AT FIFTY (50.0) FOOT INTERVALS WITH DUMMY JOINTS AT TEN (10.0) FOOT INTERVALS. WHEN ELECTRIC, GAS, SEWER OR WATER SERVICE LINES ARE IN PLACE, AN "E", "G", "S" OR "W" SHALL BE MARKED ON CURB FACE AND FLAT/GUTTER AT THE APPROPRIATE LOCATION(S). AFTER THE CURB & GUTTER HAS BEEN CURED, EXTRA PRECAUTIONS WILL BE TAKEN DURING BACKFILLING AND/OR OTHER ACTIVITIES TO PREVENT DAMAGE OR MARRING OF FINISH. REFER TO CURB & GUTTER DETAILS FOR PLACEMENT OF UTILITY MARKINGS WITH A MIN. OF 4"x2" LETTERS.
4. ALL ROADWAY MATERIALS (ASPHALT AND CRUSHED AGGREGATE BASE) SHALL COMPLY WITH THE ALDOT STANDARDS, UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
5. TACK POINTS (PRIME) SHALL BE APPLIED PRIOR TO WEARING SURFACE APPLICATION, AND BETWEEN LAYERS OF PAVEMENT MATERIAL FOR RESURFACING PROJECTS. TACK SHALL BE SS-14H OR ASPHALTIC CEMENT AC-10 OR AC-20 AS SPECIFIED BY THE CITY ENGINEER. IN ADDITION, IF PAVING OPERATION IS DELAYED, OR EDGES BECOME DIRTY OR MUDDY, ALL DIRT AND MUD MUST BE REMOVED PRIOR TO APPLYING TACK COAT.
6. BASE AND WEARING SURFACE REQUIREMENTS ARE BASED ON CBR OF 6-9 FOR SUBGRADE SOILS. ALTERNATE DESIGN FOR FULL DEPTH PAVEMENTS OR VARIANCE TO LISTED THICKNESSES WILL BE CONSIDERED ON A CASE BY CASE BASIS, BASED ON SUBGRADE SOILS AND/OR EXPECTED TRUCK TRAFFIC.
7. TREATED 1"x4" SHALL BE USED FOR EXPANSION JOINTS IN SIDEWALK.
8. FOR NON CURB AND GUTTER STREETS, A PAVED DRIVEWAY TURNOUT IS NOT REQUIRED. SLOPES MUST MEET ILLUSTRATED REQUIREMENTS ON THESE DETAILS.
9. ALL TREE PLANTINGS WITHIN THE RIGHT OF WAY MUST BE APPROVED BY THE CITY OF AUBURN PARKS AND RECREATION DEPARTMENT PRIOR TO INSTALLATION. PLANTINGS SHALL BE IN ACCORDANCE WITH THE STANDARD DETAIL.
10. WHEN A DEVELOPMENT WARRANTS THE WIDENING OF THE ROADWAY THAT WILL ALTER THE CURRENT TRAFFIC STRIPING, THE DEVELOPER, AT THEIR EXPENSE, SHALL PROVIDE A THREE-QUARTER INCH OVERLAY. THE LIMITS OF THE OVERLAY SHALL COVER ALL TRAVEL LANES AND WILL BEGIN AND END AT THE LIMITS OF THE ALL ROADWAY IMPROVEMENTS.
11. THE GUTTER DEPTH MAY BE USED TO ACCOUNT FOR THE DEPTH OF PARKING SPACE PROVIDED THERE IS A TWO FOOT GRASS STRIP BETWEEN THE BACK OF CURB AND THE FOUR FOOT WIDE SIDEWALK. IF SIDEWALK IS PLACED IMMEDIATELY ADJACENT TO THE BACK OF CURB AND THE GUTTER DEPTH IS USED FOR PARKING THE SIDEWALK SHALL BE INCREASED TO FIVE FEET WIDE. IN NO CASES SHALL THE GUTTER WIDTH BE COUNTED TOWARD THE WIDTH OF A PARKING SPACE.

SOD DETAIL



STANDARD DETAILS: STREETS - SHEET 1 of 3

PROJECT TITLE	DEPARTMENT	ENGINEERING	REVISIONS:
	SCALE:	N.T.S.	GM: 05-19-03 BS: Feb 2009
	DRAWN BY:	GINA MCCRICARD	GM: 05-21-04 GM: 06-22-11
	CITY ENGINEER:	JEFF RAMSEY	BS: 06-17-04 BS: 7-10-14
	APPROV. BY:	JEFF RAMSEY	BS: Jan 2006
	IMPLEMENTED:	JANUARY 01, 2011	BS: Nov 2007



Diagram illustrating the proposed widening of an existing pavement structure. The plan view shows the 'RIGHT OF WAY' and the 'EXISTING PAVEMENT' section, flanked by 'PPSD WIDENING' areas. The cross-section view shows the 'EXISTING UTILITY LINE' and the 'EXISTING PAVEMENT' structure, with a '3 MIN.' depth for the widening on both sides. A '5 MIN.' dimension is indicated for the widening on the left side.

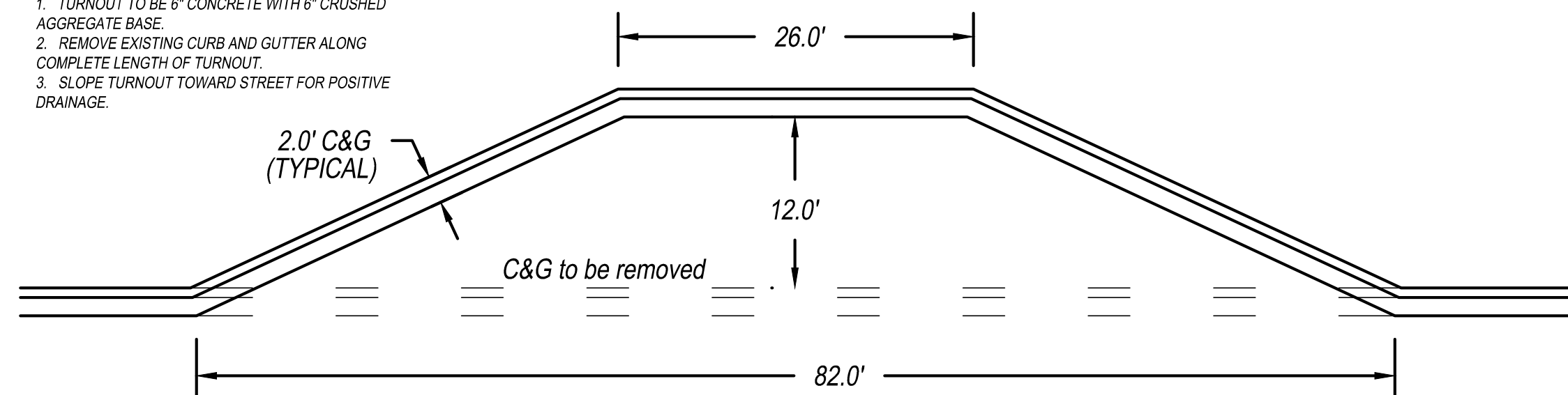
ENCASEMENT SIZING
WATER AND SANITARY SEWER

CARPENTER		SPACER		STEEL EASEMENT	
NOMINAL PIPE DIAMETER	STANDARD PIPE BELL O.D.	CASING SPACER BAND WIDTH	MINIMUM CASING THICKNESS	MINIMUM CASING DIAMETER	FEET
4	4.625	8	0.25	18	14
6	6.625	8	0.25	18	16
8	11.16	8	0.25	20	18
10	13.25	8	0.25	20	20
12	15.22	8	0.25	22	22
14	17.73	12	0.25	24	24
16	19.88	12	0.3125	28	26
18	22.16	12	0.3125	30	28
20	24.28	12	0.3125	32	30
24	28.50	12	0.3125	36	34
30	34.95	12	0.5	42	40
36	41.37	12	0.5	48	46

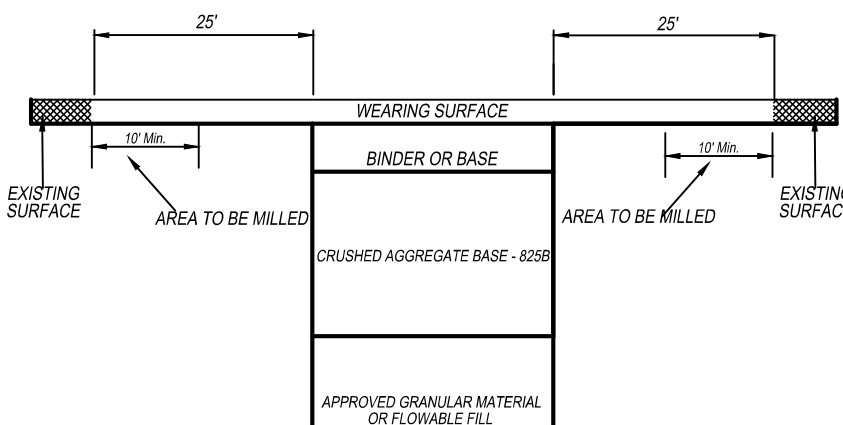
Technical drawing of a "Type "N" Special Curve". The drawing includes a side view and a cross-section. The side view shows a curved structure with a length of 15', 2 dowels at each end, and a slope of 1/4" per foot. The cross-section shows a semi-elliptical shape with a 12" radius (R1) and a 12" radius (R2). The drawing is labeled with "JOINT FILLER", "2 DOWELS", "15'", "1/4" PER FOOT", "RIDGE", "R1", "R2", "TYPE "N" SPECIAL CURVE", and "A".

NOTES:

1. TURNOUT TO BE 6" CONCRETE WITH 6" CRUSHED AGGREGATE BASE.
2. REMOVE EXISTING CURB AND GUTTER ALONG COMPLETE LENGTH OF TURNOUT.
3. SLOPE TURNOUT TOWARD STREET FOR POSITIVE DRAINAGE.



(FOR CROSSINGS PERPENDICULAR TO A ROADWAY,



NOTES:

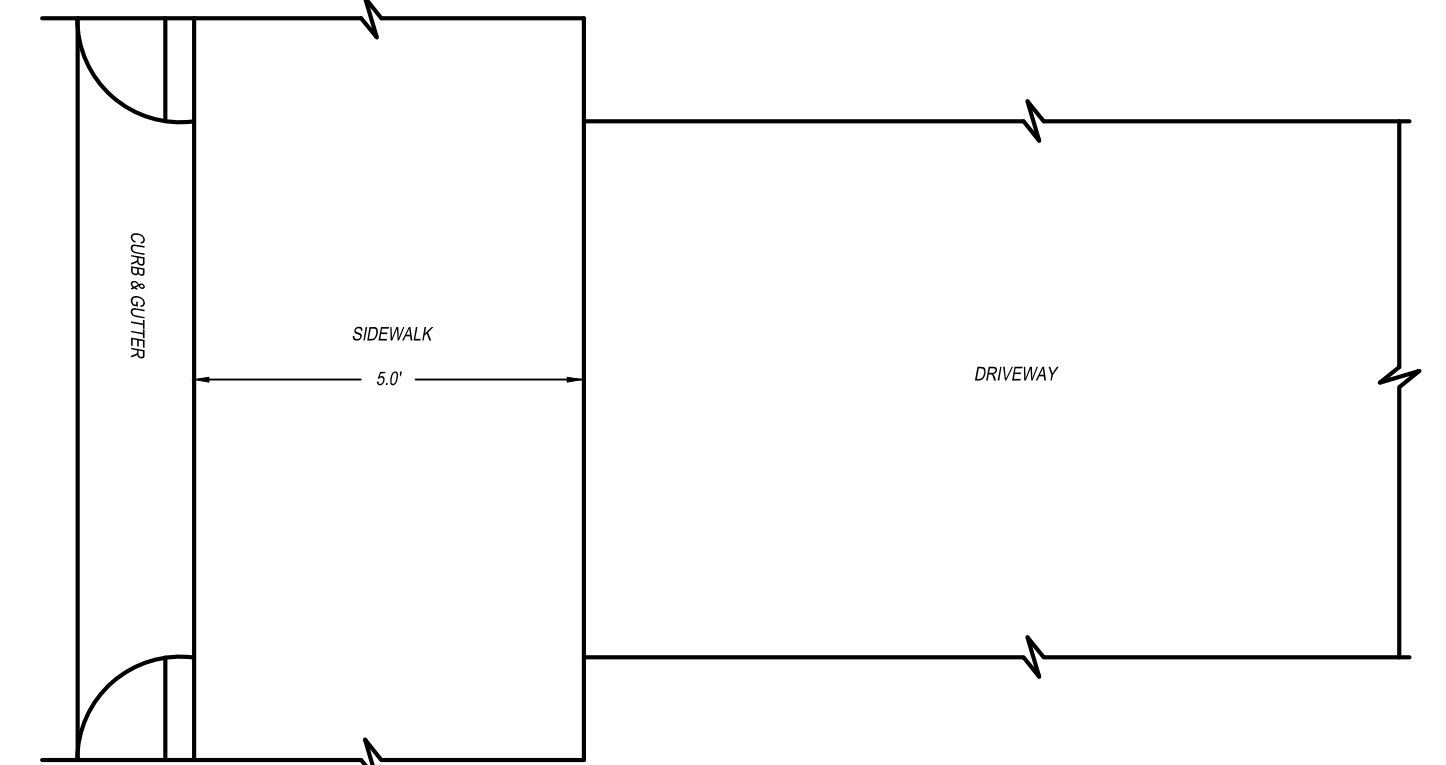
- PERMANENT PATCH SHALL BE PLACED 25' EACH SIDE OF TRENCH FOLLOWING THE EIGHT WEEK TIME. ALL TEMPORARY ASPHALT MUST BE REMOVED. ANY DEVIATION TO THIS SEQUENCE MUST BE APPROVED BY THE CITY ENGINEER. MILLING IS REQUIRED ALONG ALL EDGES AND THE OVERLAY TO MATCH ADJACENT CONDITIONS. 2" MIN. 1/4" LAVE RAVES. THE OVERLAY SHOULD EXTEND TO THE NEAREST LAKE. MILLING IS REQUIRED ALONG ALL EDGES OF PAYMENT.
- ASPHALT AND CRUSHED AGGREGATE BASE MATERIALS SHALL BE IN ACCORDANCE WITH ALDOT.
- FOR UTILITY PATCHES RUNNING PARALLEL TO THE ROADWAY, PATCHING SHALL BE THE FULL WIDTH OF THE ROADWAY. IF THE FULL 25' PATCH IS PAVED BY THE CITY ENGINEER BASED ON THE EXISTING ROADWAY CONDITIONS, THE PATCH SHALL BE EXTEND ONE (1) FOOT ON EITHER SIDE OF THE TRENCH. ALL EDGES SHALL BE SAW CUT, VERTICAL AND SMOOTH OR JACK HAMMERED AND COATED WITH TACK.

	0 TO 2,000 VPD		2,001 TO 5,000 VPD		5,001 AND GREATER	
CLASSIFICATION	ALLEY/CUL-DE-SAC LOCAL RESIDENTIAL		RESIDENTIAL COLLECTOR		COLLECTOR/ARTERIAL LOCAL COMMERCIAL	
	CLASS II		CLASS III		CLASS IV	
	FULL DEPTH	ASPHALT BASE	FULL DEPTH	ASPHALT BASE	FULL DEPTH	ASPHALT BASE
WEARING SURFACE (424A)	1"	1"	2.0"	1"	2.0"	1"
BINDER (24B) OR BASE (424C)	4"	2"	5.0"	4"	6.0"	4"
OVERSLOPE ACCUMULATE BASES (30B)	6.0"		6.0"		9.0"	

SECTION A

6.0" CURB FACE 4.7" CONCRETE 1.5" 4.5" 4' MIN. 4.0' MAX. 8.0' 1.0" / FOOT MAX. 2.5' / FOOT MAX. 1.0" / FOOT MAX. 2.5' / FOOT MAX.

The diagram illustrates a cross-section of a road with a sidewalk on the left. The sidewalk has a 2% slope. The gutter, which is the area between the sidewalk and the road, has a 4.5% slope. The road surface is shown with a 4% slope. The diagram is labeled with dimensions and slopes: 2" for the sidewalk width, 4.5" for the gutter width, 4" for the road width, and 2% for the sidewalk slope. The gutter slope is also indicated as 4.5%.



PROJECT TITLE:



DEPARTMENT:		ENGINEERING	REVISIONS:	
SCALE:	N.T.S.		GM: 05-21-04	BS: 12-9-13
DRAWN BY:	GINA MCCRICKARD		BS: 2-17-06	GM: 2-14-14
CITY ENGINEER:	JEFF RAMSEY		BS: 11-22-07	GM: 12-31-14
APP'D BY:	JEFF RAMSEY		BS: 5-28-08	
IMPLEMENTED:	JANUARY 01, 2011		GM: 06-22-11	